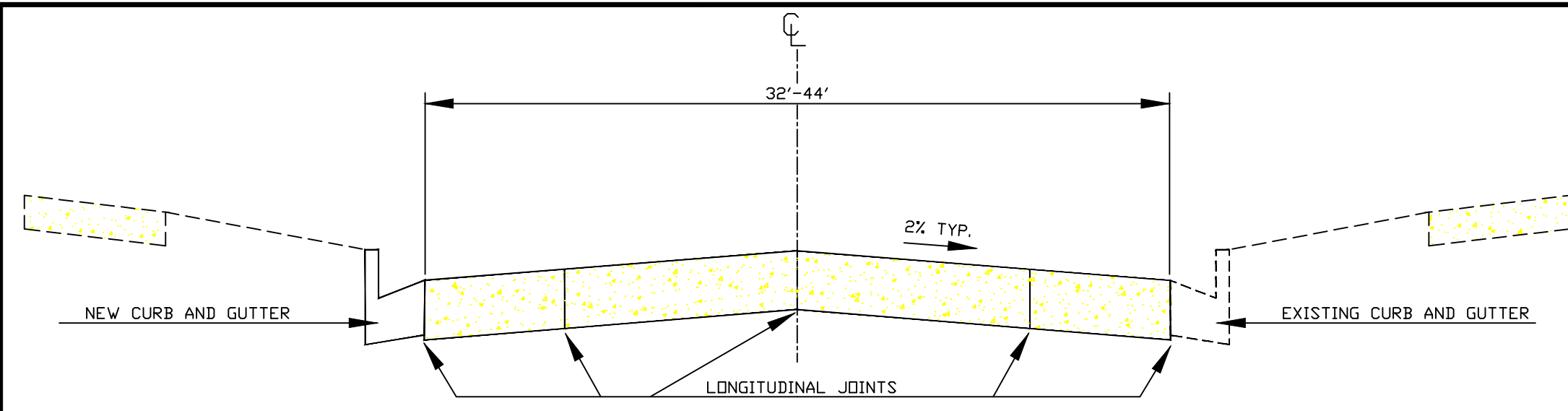
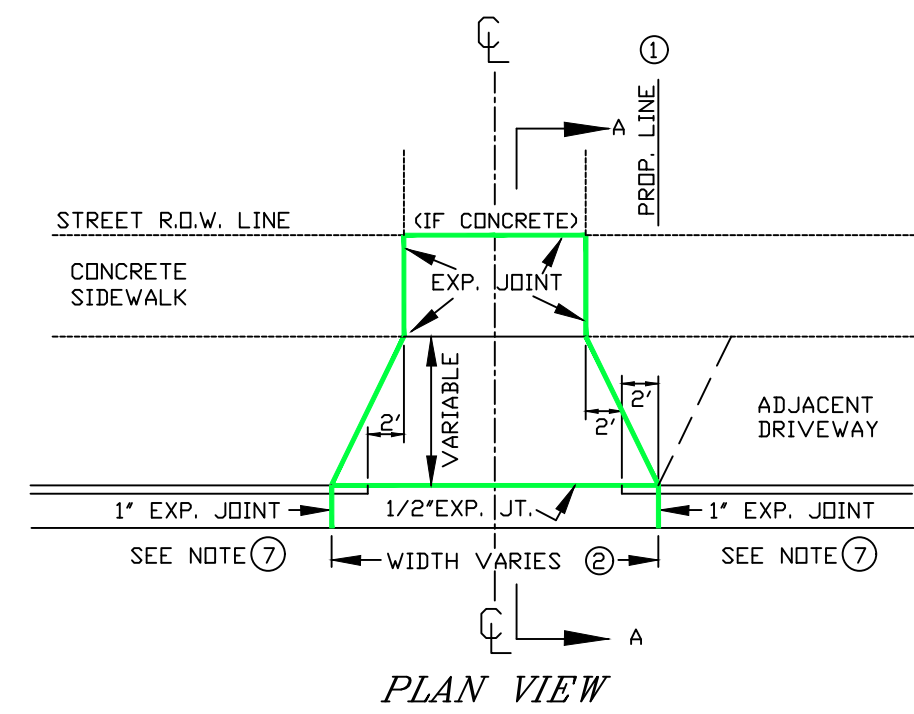


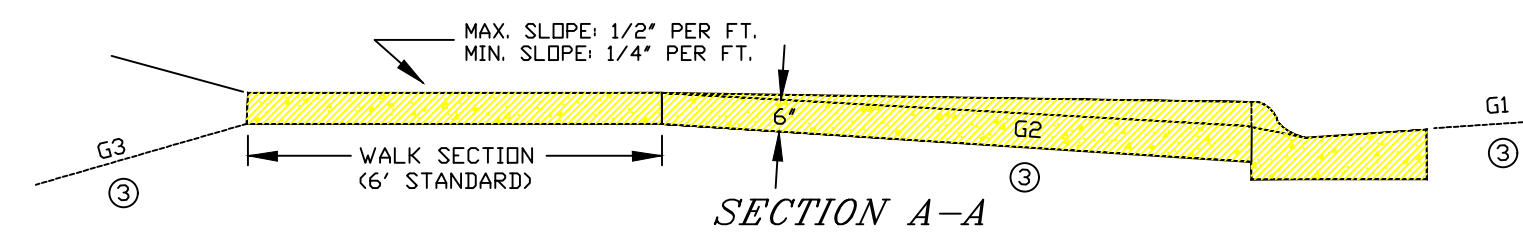
**DRAWINGS
NOT TO SCALE**



TYPICAL CROSS SECTION FOR 36' ROADWAY



PLAN VIEW



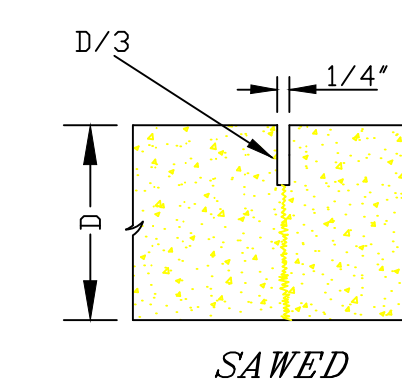
SECTION A-A

- ① DRIVE SECTION SHALL NOT OVERLAP PROPERTY LINE EXTENDED, EXCEPT WHERE PERMITTED BY THE ENGINEER, OR WHEN A JOINT DRIVEWAY AGREEMENT IS EXECUTED BY OWNERS OF ADJACENT PROPERTIES.
- ② MAX. DRIVEWAY WIDTH AT THE CURB AND SIDEWALK IS SET FORTH IN CITY ORDINANCE 5.03.
- ③ THE BREAKOVER ANGLE (CAUSE OF CARS BOTTMING) BECOMES CRITICAL WHEN THE ALGEBRAIC DIFFERENCE OF GRADES (G1, G2, & G3) EXCEEDS 11%.
- ④ A REINFORCED DRIVE SECTION IS **REQUIRED FOR CURB & GUTTER IN AREAS ZONED INDUSTRIAL OR COMMERCIAL.**
- ⑤ BACK OF CURB TO FRONT OF CONC. SIDEWALK MUST BE CONCRETE, BRICK OR ASPHALTIC BITUMINDUS.
- ⑥ **MECHANICAL COMPACTION** OF SUBSOIL IN LAYERS LESS THAN 12" TO ACHIEVE MINIMUM COMPACTION OF 95% OF MAXIMUM DENSITY FROM MODIFIED PROCTOR IS **REQUIRED.** (INCLUDING STREET SIDE AFTER FORMS ARE REMOVED)
- ⑦ EXPANSION JOINT IS REQUIRED AT BOTH ENDS OF DRIVEWAY WHEN ONLY DRIVEWAY IS INSTALLED OR REPLACED. WHEN ENTIRE BLOCK OF CURB & GUTTER IS INSTALLED THE EXPANSION JOINT AT DRIVEWAY ENDS MAY BE OMITTED.

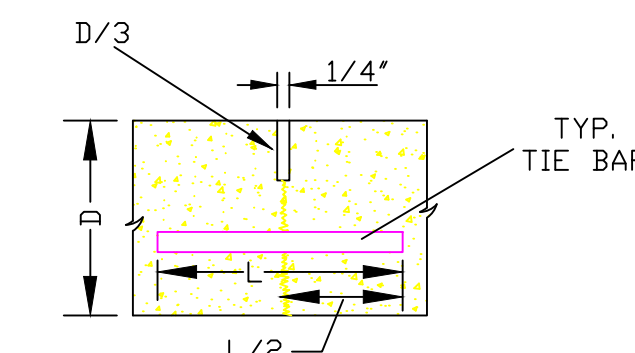
NOTE: TURNING OF 2" DIAMETER DRIVEWAY RETURNS IN LIEU OF DIMINISHING HEAD AS SHOWN IS PERMITTED IF DESIRED BY PROPERTY OWNER. INSTALLATION OF A DRIVEWAY BY REMOVING EXISTING CURB HEAD ONLY IS NOT ALLOWED. ENTIRE EXISTING C&G MUST BE REMOVED FOR NEW DRIVEWAYS. REMOVAL OF A MINIMUM 12" WIDTH OF BITUMINDUS TO INSTALL FRONT FORMS IS REQUIRED.

STANDARD DRIVEWAY DETAIL

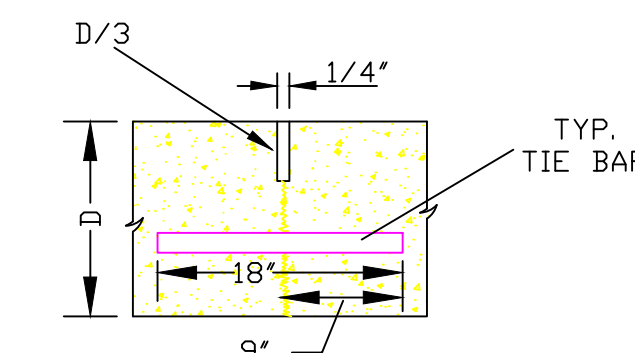
CONTRACTION



SAWED

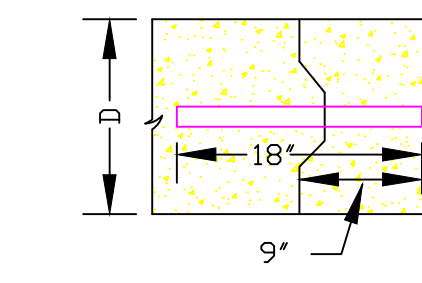


TIED LONGITUDINAL

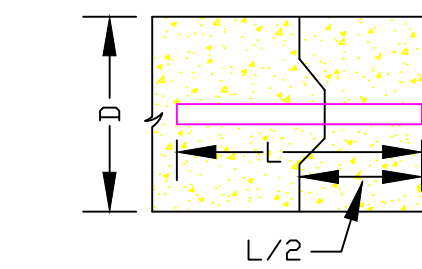


DOWELED TRANSVERSE

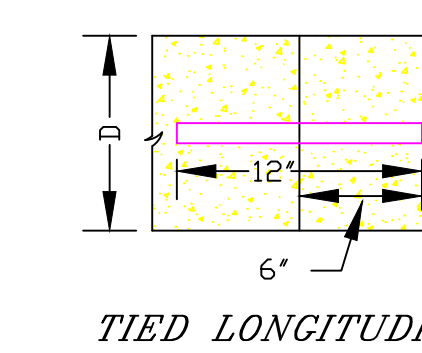
CONSTRUCTION



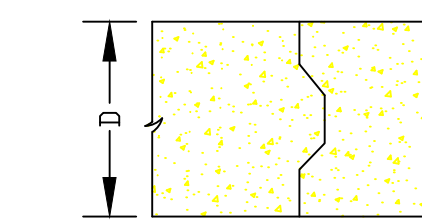
DOWELED TRANSVERSE



TIED LONGITUDINAL

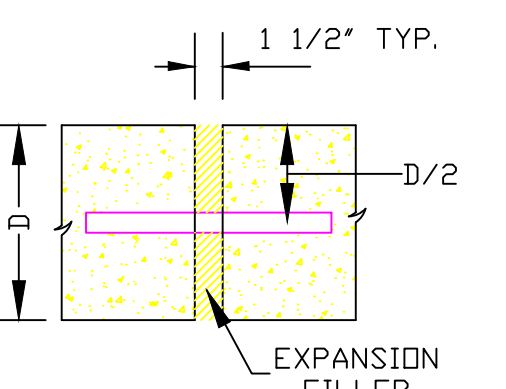


TIED LONGITUDINAL



UNTIED

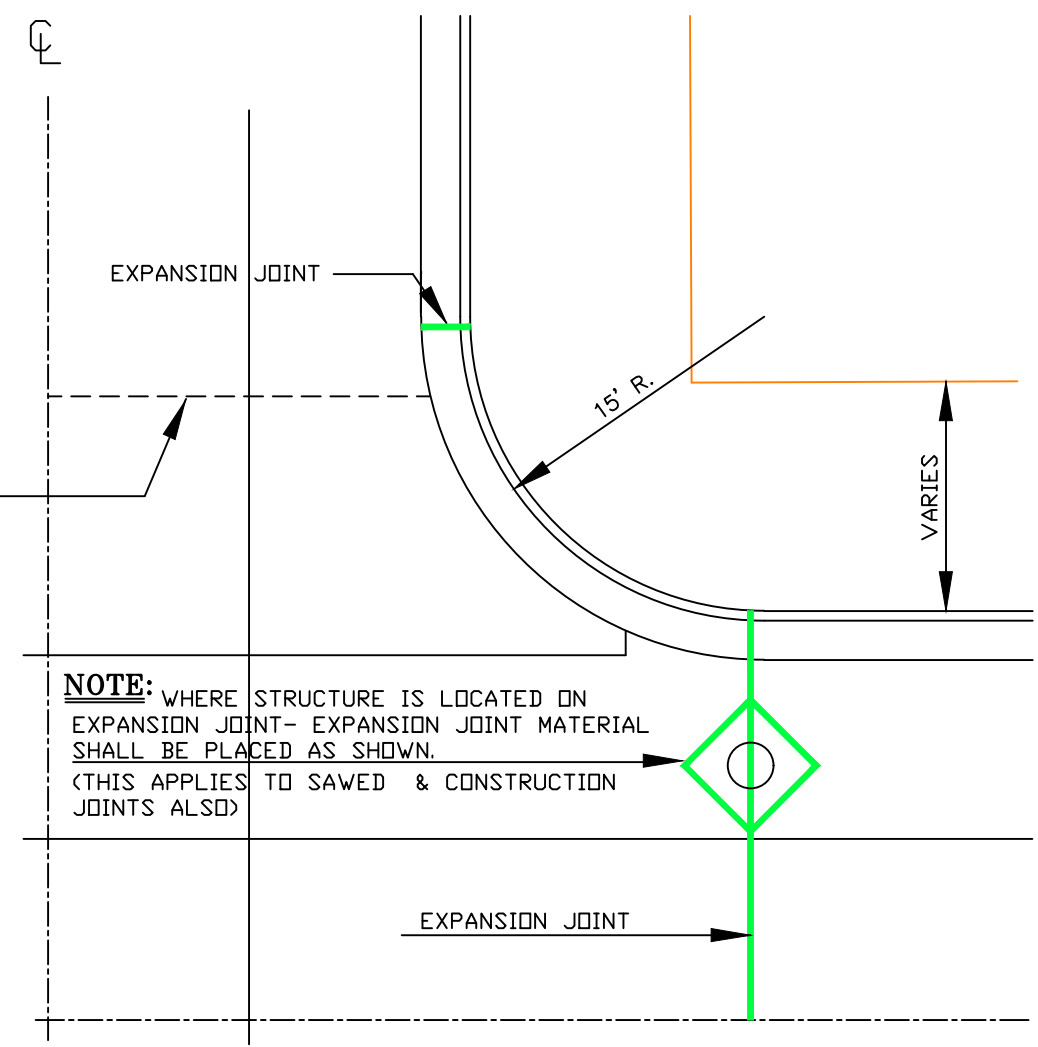
EXPANSION



DOWELED TRANSVERSE

ALL JOINTS TO BE SEALED TO KEEP OUT FOREIGN DEBRIS

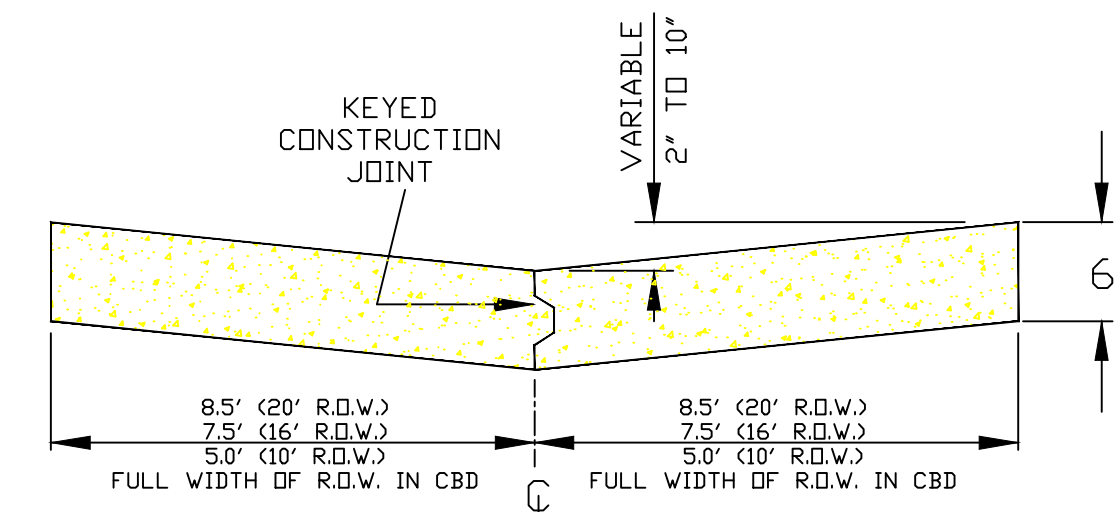
NOTE: IF SIDE STREET IS NOT TO BE PAVED, END PAVEMENT AT STREET LINE.



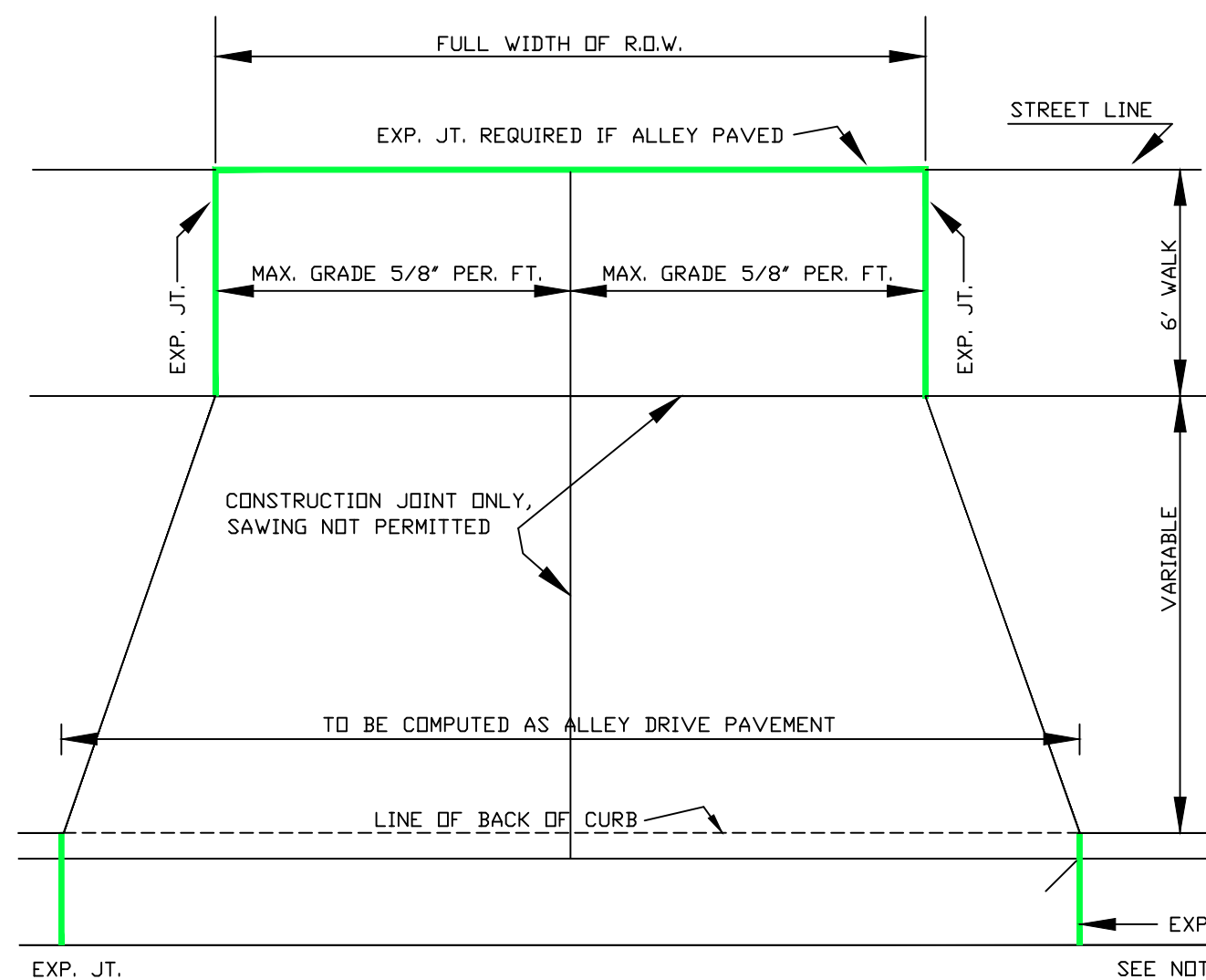
1/4 TYPICAL INTERSECTION

NOTES-JOINTS

1. EXPANSION JOINTS SHALL BE PLACED AT THE END OF RADII AT STREET INTERSECTIONS AND MID-BLOCK BETWEEN INTERSECTIONS. IN NO CASE SHALL THE DISTANCE BETWEEN EXPANSION JOINTS EXCEED 160 FEET.
2. THE DISTANCE BETWEEN TRAVERSE JOINTS SHALL NOT BE LESS THAN 10 FEET AND SHALL BE TYPICALLY 20 FEET APART.
3. LONGITUDINAL CONSTRUCTION JOINTS BETWEEN CURB AND GUTTER SECTION AND CONC. PAVING SECTION SHALL BE SEALED AS SHOWN ON JOINT DETAILS.

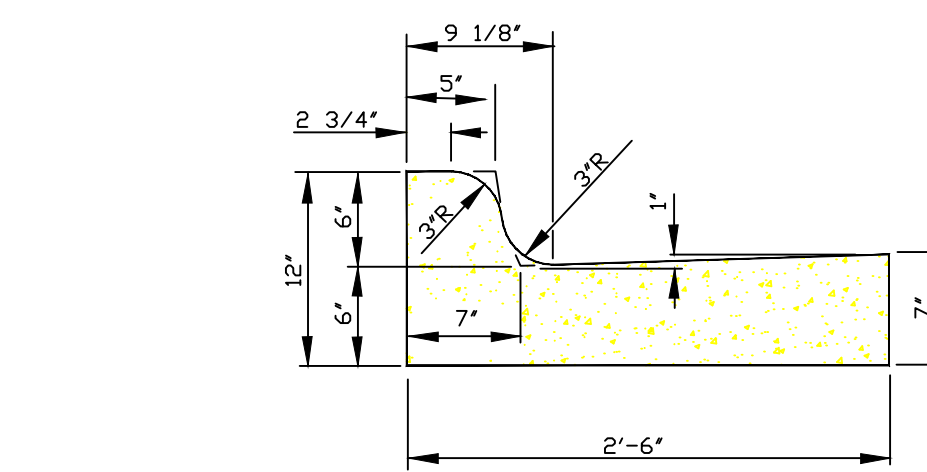


TYPICAL SECTION OF ALLEY PAVEMENT



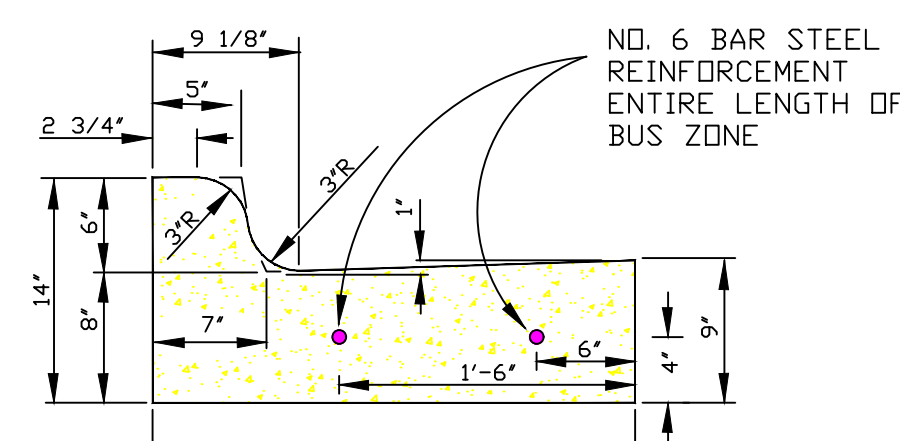
STANDARD ALLEY DRIVEWAY 7" THICK

NOTE: SLOPE TO WALK IN PLACE AT MAX. 3/4" PER FT.

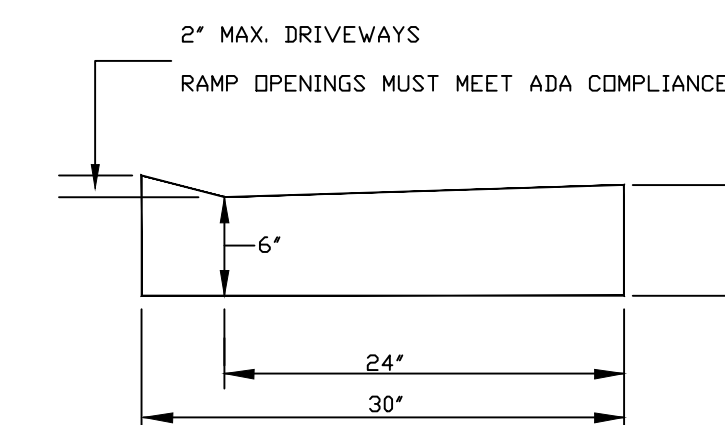


STANDARD CURB & GUTTER SECTION

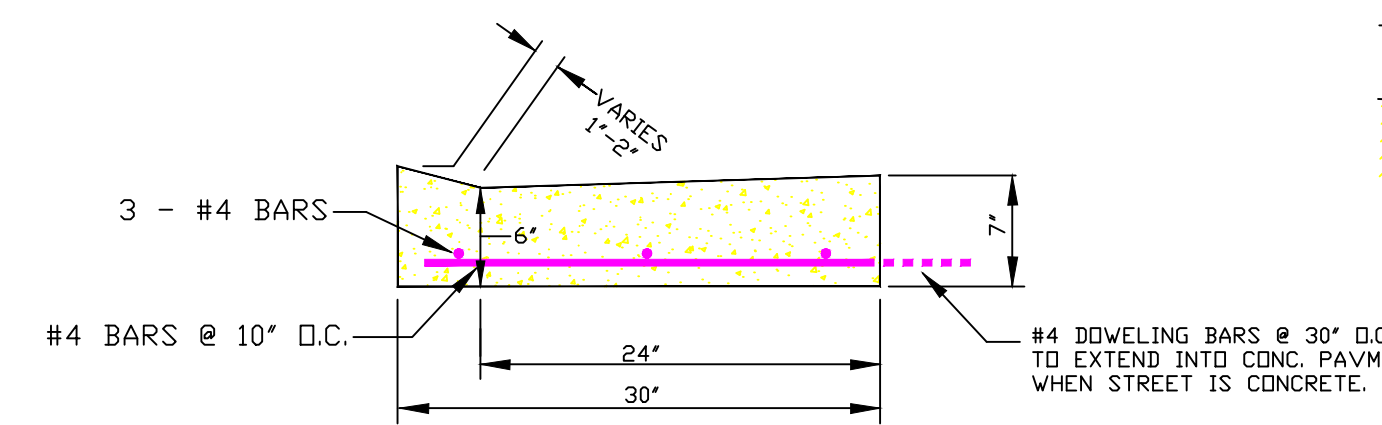
NOTE: WITH REFERENCE TO SAWING OF CONTRACTION JOINTS ON SLIP FORM CURB & GUTTER, & CURB, PAGE 9.3, STANDARD SPEC'S THE SAW CUT SHALL BE A MINIMUM 1/8" WIDE X 1" DEEP.



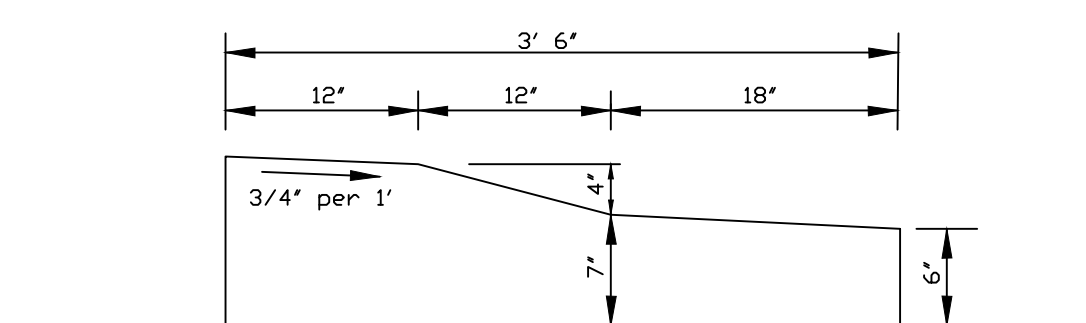
REINFORCED CURB & GUTTER BUS STOP LOCATIONS



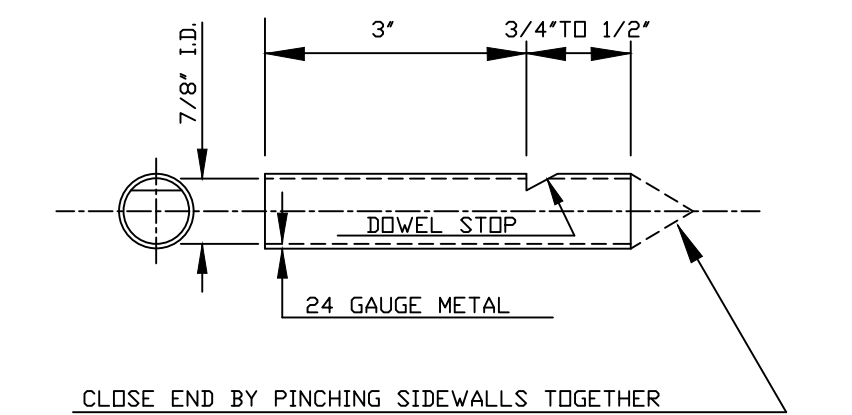
MOUNTABLE CURB SECTION



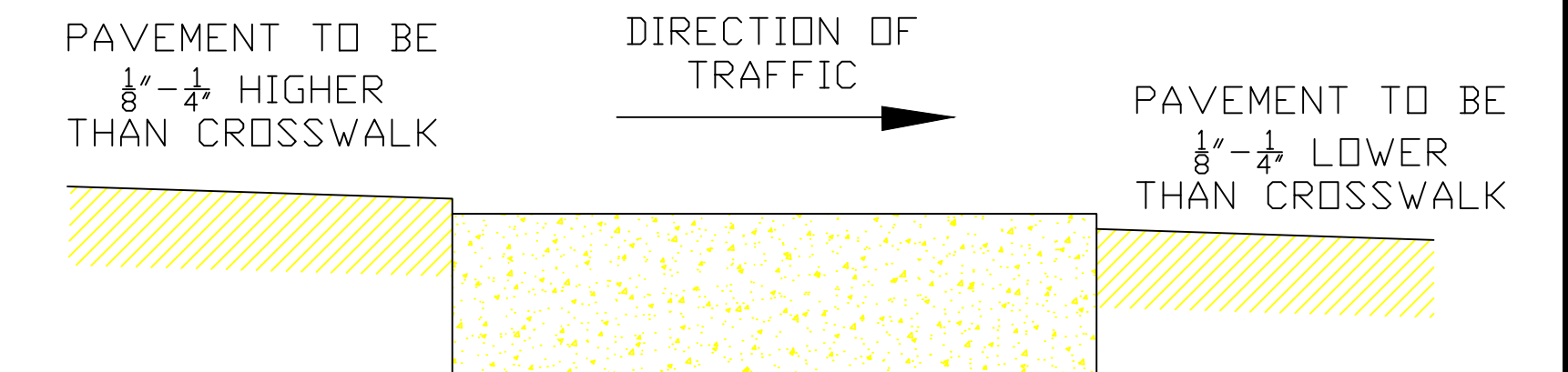
REINFORCED DRIVEWAY



TRAFFIC CIRCLE CURB SECTION



DOVEL SOCKET DETAIL



CONCRETE CROSSWALK DETAIL

PROJECT No.	CONCRETE-PAVEMENT DETAILS		
LOCATION			
RESOLUTION	DATE		
ENGINEERING DEPT. City of LaCrosse, Wis.			
FIELD BOOK	SURVEYED	BY	DATE
NUMBER	DRAWN	PRELIMINARY	
	CHECKED	FINAL	
	APPROVED		
PAGE	REVISIONS		01/21
SCALE: NONE			
SHEET NO.	TOTAL SHEETS		